

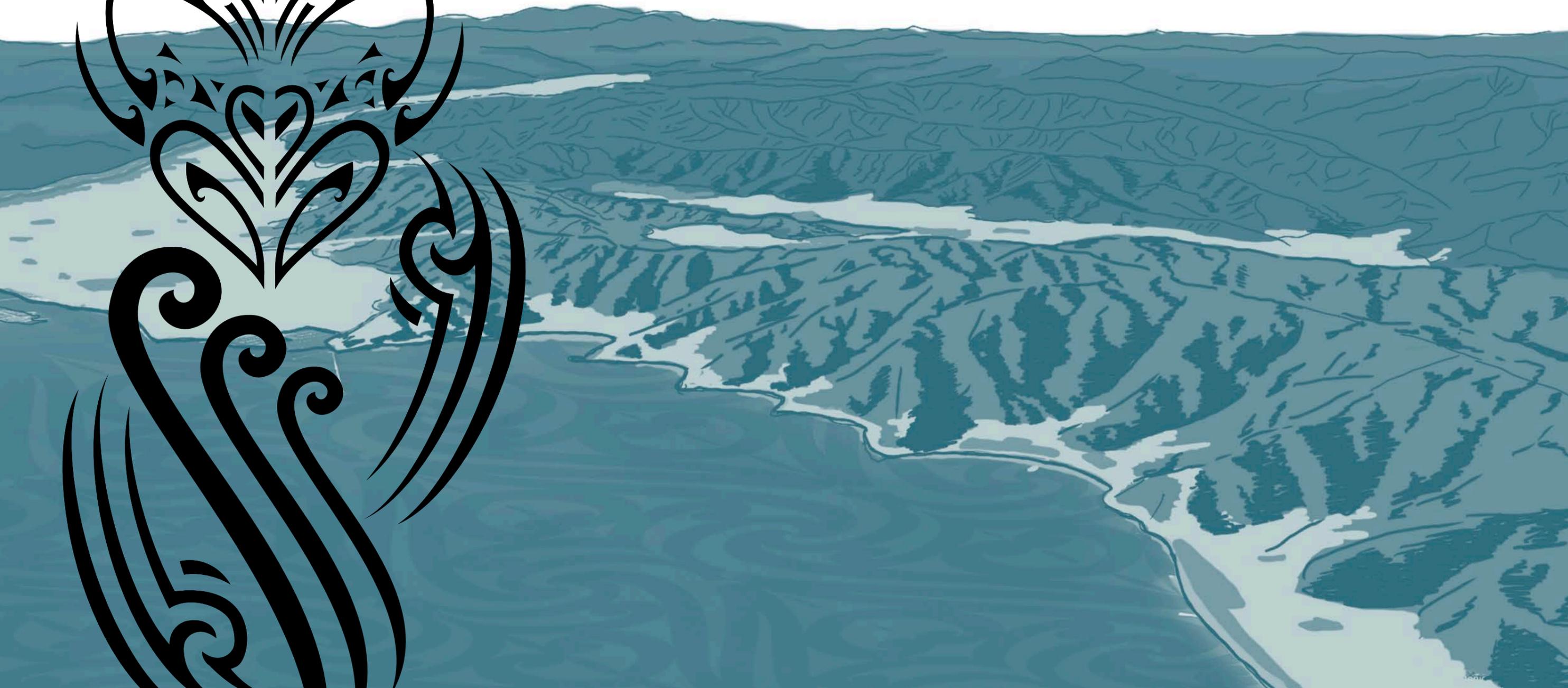
# Tupua Horo Nuku.

Sunshine Bay - Urban Design Plans.  
Eastern Bays Shared Path

22 July  
2022



**Te Ara Tupua Alliance**  
Shifting gear to connect past, present and future



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# Tupua Horo Nuku. Eastern Bays.

The Eastern Bay area encapsulates many wahi tapu from Te kongutu o Te Awa Kairangi to Te Waha o te Ikanui. Its beginnings emanate out of the power and mana of Tupua-horo-nuku (evolving mass of solid matter), known as the tupua, Ngake.

Instructed by the mountain clan people who were summoned to the head of the fish, gathering on Pukeatua where they were gifted the appropriate incantations to prise open the mouth of the great catch of Māui-tikitiki-a-Taranga to enable it to breathe again, where they summoned from the great depths of Rua Tupua and Rua Tawhito of the fresh water lake who brought forth Tupua-horo-nuku and Tupua-horo-rangi.

Tupua-horo-nuku, Tupua-horo-rangi  
Tai kukume mai takiwā ia mouri e runga  
Kia horo wawe mouri e raro koi ikaroa<sup>1</sup>

The narrative of the eastern bay speaks of and highlights “te ihi, te wehi me te mana nui o Tupua-horo-nuku.”

Te Awa Kairangi, formed out of the raging whip lashing tail of Ngake as he wound himself up into a frenzy, generating and amassing energy and power, splitting the land mass immediately behind him lacerating Papatūānuku, imbuing “te ara mouri” inland to the Tararua and Remutaka. Whilst at the same time hurling himself towards the barriers hearing the pounding and thunderous waves smashing in the distant. Smashing his way out from his land lock imprisonment to freedom unto Hinemoana and Tangaroa. In his destructive escape came forth the islands of the harbour later to be named by Kupe the pacific navigator, and as centuries passed the peopling of Te Wai-manga arrived gifting new names later to be suppressed through imperialistic and colonial methodologies which are still impacting on us since their arrival in 1769.

Tēnei te ara kei runga  
Tēnei te ara o Ranginui e tū nei  
Tēnei te ara o Papatūānuku e takoto nei...<sup>2</sup>

Ripiripia te ika nui  
Haehaea te ika roa  
Ka hora, ka hora te kai ki a Tamanuiterā  
Ka hora, ka hora te kai ki a Tāwhiri-mātea...<sup>3</sup>

1 He karakia nō te kainga  
2 He karakia nō te kainga  
3 He karakia nō te kainga

Immediately following the severing, Hine-wai-tootaa and Hine-kōrako went about their duties caressing and gently healing Papatūānuku. Calling upon their sister Hine-wairere they asked her if she could follow the scarification marks of Papatūānuku until she was fully covered to sooth her skin to ease the pain. To this day they still nurture and care for her.

Te Awa Kairangi like many rivers began its life through the kuia Hine-wai-tota, Hine-kōrako and Hine-wairere, being the ancestress of condensation, lunar droplets and water flow gathering on the many peaks on both sides of the river. Fed by melting snow, ice and rainwater running off the land, the collective of droplets follows cracks and crevices within the landscape formed out of the raging whip lashing of the tail of Ngake (seismic activity) in his attempt to escape to freedom from his land lock lake imprisonment.

The many small tributaries joining together growing larger forming the collective mass of Te Awakairangi, flowing every second of the day. The following whakatauaaki encapsulates who the people of Te Ātiawa are and our responsibility for the water and the whenua.

**Te Ātiawa tupua rau, he auripo i te manga iti, he auripo i te manga nui  
rānei, he kaitiaki ki te whenua <sup>4</sup>**

Te Ātiawa of many phenomena's, where there is a ripple in a small tributary or great river, there is a guardian and protector on the land.

Over time the continuous flow of Te Awa Kairangi has shaped the landscape moving and wearing away rock, carving out a network of valleys eventually reaching the lower grounds, widening and reaching the point where the fresh water meets the salt water.

Whakapakarukaru puare te waha o te ika roa Te hononga o ngā wai e rua...<sup>5</sup>

The Eastern Bay commences at the meeting of the waters.

4 Nā Kura Moeahu whakahī  
5 He karakia nō te kainga



Figure 1.1 Tupua-Horo-Nuku artwork.  
Len Hetet, 2021

# Tupua Horo Nuku. The Pathway.

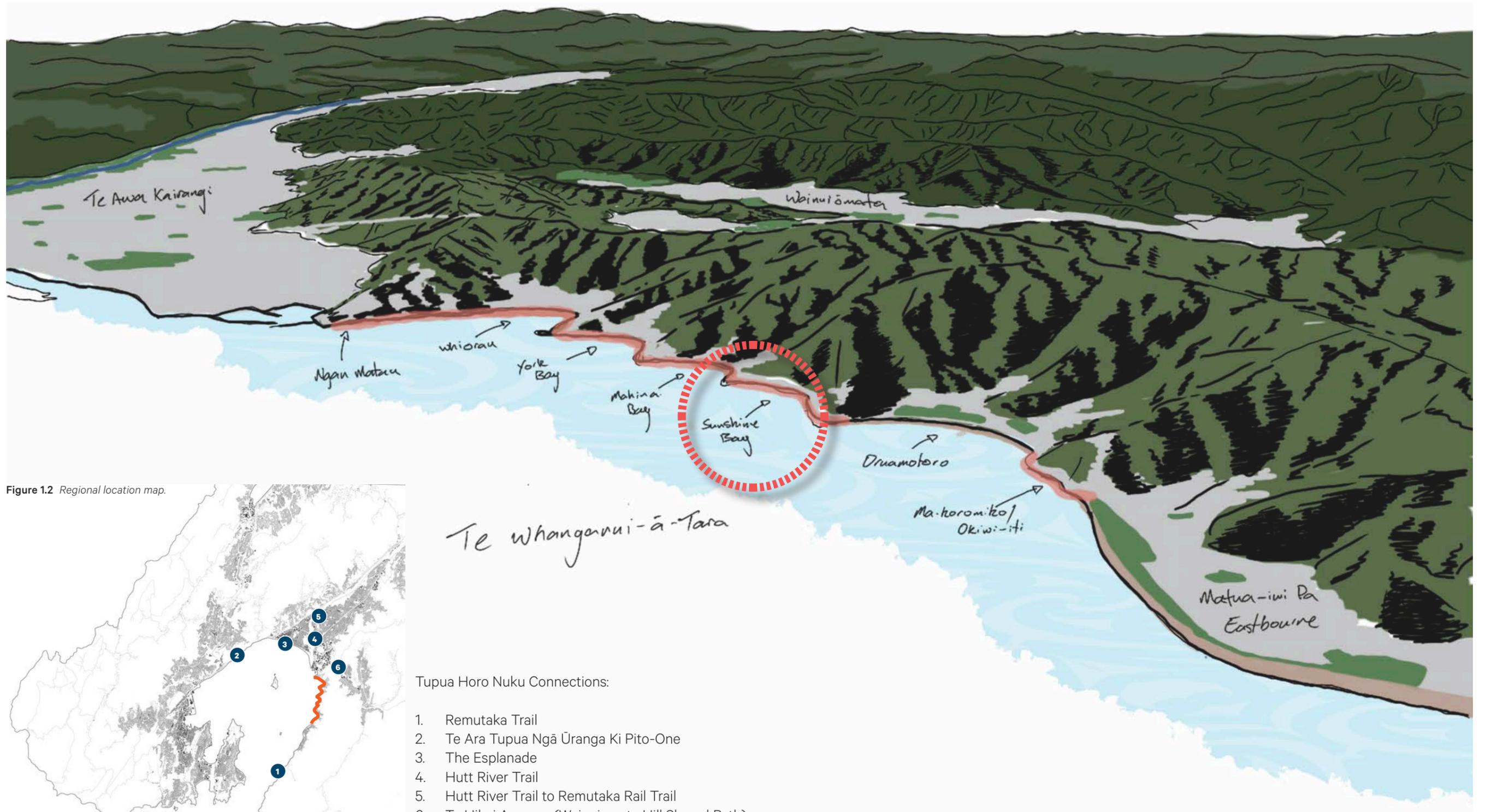


Figure 1.2 Regional location map.

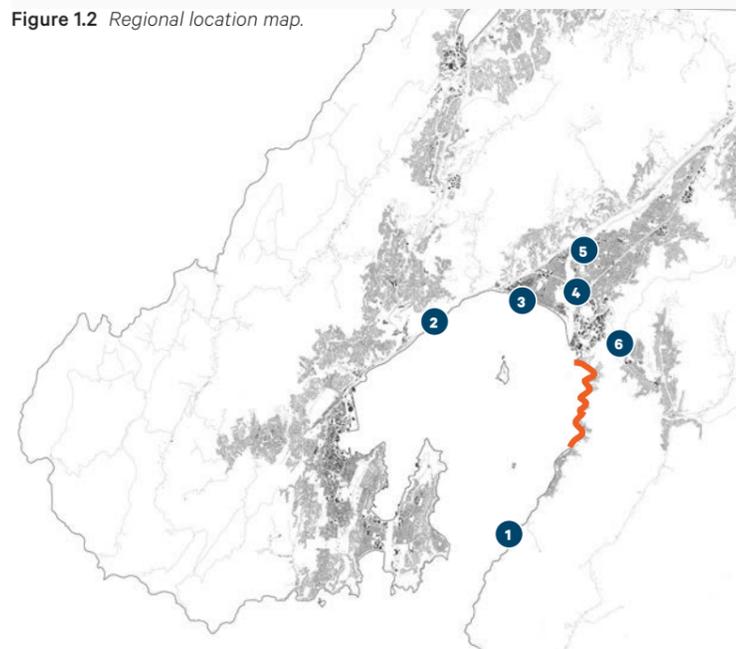


Figure 1.3 Tupua Horo Nuku context.

# Introduction.

## Purpose & Objectives

The purpose of the Bay Specific Urban Design Plan (BSUDP) is to provide bay specific detailed design for the project responding to local landscape character, identity and land use, in the broader context of the Landscape and Urban Design Plan (LUDP).

Consent conditions for the Eastern Bays Shared Path (Tupua Horo Nuku) guide the content of the BSUDP and the preceding LUDP. The conditions outline the purpose of the management plans, the contents of the plans, expert inputs, stakeholders to be consulted, the approval and certification process and how to manage disputes. Consent conditions are listed on page 9.

Condition LV.6 requires the BSUDP to be submitted in two stages.

Stage 1 is a draft design protocol. Building on the overall design approach and narrative set out in the LUDP. The bay specific design protocol describes the special landscape and natural character of each bay and outlines the aesthetic principles to be applied. Community comment on the Sunshine Bay was sought on the draft design protocol.

Stage 2: The final BSUDPs are to be certified either on their own (in accordance with Condition GC.5) or, if included in the initial LUDP, when the LUDP is certified under Condition LV.1. Community aspirations, engineering constraints with urban design and landscape layers are applied to achieve a unique bay specific design, integrated with the Eastern Bays Shared Pathway.

## Structure

In satisfying Conditions LV.5 - LV.7 of the Resource Consent the Draft Design Protocol process informing the BSUDPs is:

- Illustrate bay locations and describe the landscape context.
- Ascertain draft priorities for each bay. Priorities include issues involving: safety, access and mobility; engineering; ecology; natural character; landscape; urban design; and recreational and amenity elements.
- Show visual representations of best practice through precedent examples of comparable shared path projects.

- Outline landscape and urban design approaches and principles for each bay to set the scene for design plans and details.
- Develop an illustrative schematic plan for each bay.

# Consultation.

## Summary of Consultation Process

The consultation on the bay-specific design was carried out in accordance with the conditions of the resource consent for the project, specifically condition LV.6 which describes a two-stage approach for developing the Bay-Specific Urban Design Plans for each bay.

The condition requires that the draft design protocol is provided to: “the relevant Resident Association (if any) for the affected bay, the East Harbour Environmental Association, and the Eastbourne Community Board within 15 working days of receipt”.

The draft design protocols were issued on Monday 14 March to:

- Residents representatives of Sunshine Bay and Mā-Koromiko (neither bay has a formally established Resident Association, but named individuals have previously acted as bay representatives)
- The Eastbourne Community Board
- East Harbour Environmental Association

Each was requested to provide comments, if any, by Sunday 3 April (15 working days following issue).

Comments were made by:

- Residents from Mā-Koromiko on the Mā-Koromiko design protocol
- The Eastbourne Community Board
- East Harbour Environmental Association

No comments were made by residents from Sunshine Bay on the Sunshine Bay design protocol.

Condition LV.6(a) requires that the comments and the consent holder’s responses including whether the comment is accepted and reasons if the comment is not accepted be provided, within 20 working days. Following the receipt of comments, the project team has considered all comments raised and created a response. We have outlined below whether the key points of each comment are accepted or not accepted. The final Bay Specific Urban Design Plans will be developed in line with the direction explained below.

**Summary Table.**

<i>Comment Title</i>	<i>Raised by</i>	<i>Description</i>	<i>Project Team Response</i>
Beach ramp access	Community Board	Comment that Sunshine Bay beach access (and other bays with beaches) should incorporate ramps to enable wheelchairs, kayaks and prams to access the beach.	<b>Not Accepted</b> The space required to install a ramp, particularly to wheelchair access standard, to the Sunshine Bay beach would result in a significant encroachment on the beach itself. There is alternative beach access at Days Bay serving the southernmost bay communities. Ramps will be considered for beaches in the bays further north.
Sunshine Bay crossing	Community Board and EHEA	Comment that the location of the crossing at the southern end of Days Bay is unsuitable and that the shared path should be on the seaward side of Days Bay	<b>Not Accepted</b> The location of the crossing represents the extent of the project. The sight distances to the crossing from the corner currently comply with the Waka Kotahi Pedestrian and Cycling Network Guidance for the existing speed environment of the area and the crossing has been positioned in the optimum location to achieve this. Hutt City Council is reviewing speed limits in the area to improve safety. We note the long-term aspiration for a shared path that continues on the seaward side of Marine Drive through Days Bay.
Penguin sanctuary	Community Board	Comment that the penguin sanctuary is not shown in the schematic drawing, and that this is of importance to the community.	<b>Accepted</b> We note the location of the Penguin Haven at the point between Days Bay and Sunshine Bay. We will note the location of this on the final plan. There will be measures in place to avoid impacting the penguin haven during delivery in the area.
Bus stops	EHEA	Comment that the bay specific design protocols should show the detailed design for bus stops in order to meet the consent conditions. Comment that the Sunshine Bay bus stop should include a footpath in front and cyclists route behind the shelter structure.	<b>Accepted.</b> The bay-specific urban design protocols and plans do reflect the priorities required in the conditions. In the case of the Sunshine Bay bus stop the shared path will pass behind the bus stop while a footpath with a raised kerb in front of the bus shelter will enable walking and boarding / alighting from buses.
Way-Finding Signage	Community Board	Question about the design of way-finding signage and comment requesting that this be clear with high-contrast and high visibility.	<b>Accepted</b> There will be directional signage in line with Waka Kotahi MOTSAM standards and cycle network guidance. This is an addition to 'architectural' features such as the Mouri Markers.
Placement of Street Furniture	Community Board	Question about the specific locations of signs, bins, bike stands, and seats and comment that these need to be placed to avoid impacting shared path function.	<b>Accepted</b> Placement of all street furniture is already planned to avoid obstruction to the shared path. Please note that the design has been developed to have a low-profile coastal aesthetic and avoid 'clutter'. As a result, these features are relatively few in number and placed selectively. For example, rubbish bins are minimised to encourage people to take rubbish away with them and to avoid rubbish being blown out of bins.
Use of 'Landing' for Walking	Community Board	Question about whether the landing can be used as space for walking, and comment that beach access steps should provide access for walking on the landing.	<b>Not Accepted</b> The landing in the seawall profile is not designed for walking. The beach access steps are designed to discourage access onto the landings. However, access will not be prevented and it is acknowledged some people will use the landing for walking or sitting.
Water Fountains	Community Board	Question about where water fountains will be located	<b>Not Accepted</b> There are no water fountains proposed within the Sunshine Bay or Mā-Koromiko sections. Alternative water fountains already exist in Days Bay.
Lighting	Community Board	Question about the outcome of assessment of lighting for the shared path.	<b>Accepted</b> The lighting assessment has been completed. Mostly existing lighting will be used. To bring the new path up to the appropriate lighting standard some existing light fittings will be replaced, and 2 additional poles will be installed near the carparks at the southern end of Sunshine Bay. These will be located clear of the shared path and provide adequate lighting for path users.

# Consultation.

## Summary Table.

<i>Comment Title</i>	<i>Raised by</i>	<i>Description</i>	<i>Project Team Response</i>
Nibs/ Separators	EHEA	<p>Comment that the cross section of the nibs should include a concave curve facing the path and that their widths should be narrowed.</p> <p>Questions about the material selection - Whether to be hardwood or concrete.</p>	<p><b>Not Accepted</b></p> <p>The shape has been selected to provide a standard design that balances cost, future maintenance and safety. The materials for these will be timber for the seaward side (specifically, treated pine) and concrete for the road side. Concrete is selected for the road side in order to act as a kerb, helping to exclude vehicles from the shared path.</p>
Wheelchair Accessibility	EHEA	<p>Comment that all parts of the shared path must be wheelchair accessible in line with Building Code D1.</p>	<p><b>Accepted</b></p> <p>The design will meet the D1 standard. There are no ramps within the two bays sections however the rest of the path will meet these guidelines.</p>
Slip resistance and algae	EHEA	<p>Comment that stormwater outfalls should be designed so that algae will not grow on the seawall ledges.</p>	<p><b>Not accepted</b></p> <p>Stormwater outlets will be a like-for-like extension of the existing stormwater pipes on the current vertical alignment. In some places this means that stormwater will flow across the landing. The landing is not intended as a walking area. The landings include a textured surface to promote organic marine growth and biodiversity. The potential for organic growth at the culvert locations is not specifically addressed.</p>
Stairs accessibility	EHEA	<p>Comment that the seawall steps should meet the Building Code D1 standard for accessible stairways, or an explanation given as to the accessibility of design of the stairs.</p>	<p><b>Accepted</b></p> <p>The handrails on stairs meet the requirements of D1 of the Building Code.</p>

# Relevant Consent Conditions

## LV.5

The LUDP shall include the final BSUDPs for each bay within the Project area. The final BSUDPs shall address detailed design within the particular bay for the benefit of pedestrians, cyclists and others using the local road network as well as the specific urban design, landscape, ecology and recreational amenity matters (including those listed in Condition LV.7) as relevant to the particular bay.

The final BSUDPs may be prepared later and added to the LUDP on a staged basis if the Construction Works are staged bay by bay and individually certified under Condition LV.6.

## LV.6

The BSUDPs shall be prepared by the Consent Holder in two stages for each bay:

### (a) Stage 1:

A draft design protocol that sets out the priorities for the bay design in terms of engineering, safety and access and mobility requirements as well as ecology, natural character, landscape, urban design and recreational amenity elements and issues. The draft design protocol shall provide visual representations of best practice on comparable coastal shared path projects to demonstrate the level of design to be targeted. The protocol shall be provided to the relevant Resident Association for the affected bay (if any) The East Harbour Environment Association and the Eastbourne Community Board for comments (if any) within 15 working days from receipt.

Any comments received, and the Consents Holder's response and reasons if they are not accepted, are to be provided to the Manager, Environmental Regulation, and Team Leader, Resource Consents alongside the draft design protocol, within 20 working days from receipt of the comments.

### (b) Stage 2:

The final BSUDPs are to be certified either on their own (in accordance with Condition GC.5) or, if included in the initial LUDP, when the LUDP is certified under Condition LV.1.

## LV.7

The BSUDPs shall include specific landscape and urban design details for:

- (a) Seawall structures, including transition zones between seawall types and transitions between natural or rocky areas and seawall structures;
- (b) Beach access including steps, ramps and associated handrails where required, **so that people wishing to access the beach can do so safely;**
- (c) Safety barriers and railing and screening barriers between important habitat for Shoreline Foragers and the shared path;
- (d) The treatment of stormwater structures at the coastal interface;
- (e) Little Penguin and Shore Forager related structures including penguin passage elements, ramps, nests, boxes and wooden poles for roosting;
- (f) Planting treatment;
- (g) The treatment of existing trees and existing landscape and natural features;
- (h) The design and area of space available for recreational amenity activities;
- (i) The design and orientation of features, spaces and access points;
- ~~(j) The design of bus stops/shelters to enhance safety and minimise risk for all users of the Shared Pathway and the road;~~
- (j) ~~(k)~~ Refuge and seating opportunities, including size and arrangement of space to allow for stopping and gathering at frequent intervals distributed along the route;
- (k) ~~(l)~~ Signage ensuring their consistency along the shared path, including branding and reduction of visual clutter;
- (l) ~~(m)~~ Storyboards;
- (m) ~~(n)~~ Surface treatments; **and**
- (n) Any other relevant matter for that bay necessary to achieve the purposes of the LUDP in condition LV.2.

# 1. Urban Design Plan.



# Sunshine Bay - Urban Design Plan.

## Sunshine Bay is characterised by:

Sunshine Bay is a wild, exposed landscape, with a more random settlement pattern than other bays and appears less developed. The road is contained between the coastal escarpment and coastline. In some areas there is a visibly eroding road edge. There is a moderately sloping gravelly beach just below the road, with long stretches of rock outcrop along the coastline.

Sunshine Bay, opposite the Sunshine Service Station is one of two sites along the route where there is no current significant retaining structure existing between the beach and the road. The adjacent road edge has been frittered away and there are remnants of previous structures and rip rap material that has been placed over the beach.

Sunshine Bay is a shallow bay, angled rather than curved, with a narrow beach at the base of a steep coastal escarpment. The landscape is wilder than the other bays, more exposed and with distinctly coastal vegetation. The bay is sparsely populated with a few houses tucked into the base of the landforms, concentrated at the north and south ends of the bay. Development is more prominent at the south end of the bay with a cluster of buildings around the Sunshine Services Station along Marine Drive and houses on Ferry Road in Days Bay visible along the top of the escarpment.

Marine Drive is sealed to the residential boundary apart from a few gravelled parking bays. There is a strong residential character to the edge of the road, with garages, driveways, low retaining walls and a mix of amenity planting interspersed with pohutukawa and native coastal re-vegetation.

The current path runs along the coastal edge. The path width varies from 0.5 to 2m, generally being wider towards the headlands and narrower in the middle of the bay where the seal has eroded and the beach runs up to almost the edge of the road. Towards the north end of the bay the edge has been retained and possibly built up to form a small lay-by, with 2no. pohutukawa. This lay-by is scarcely big enough for a single car to park.

Street lights are located on the inland side of the road, and power poles on the coastal edge or even on the beach.

The northern headland between Sunshine and Mahina Bay is minimal, a small gravelled area, with low rocks below road level. There is a single pohutukawa tree. The southern headland is larger. It has a pumping station on it, well set back from the road and surrounded by planting and a timber fence. The rock outcrops and landforms extend further to the south heading into Days Bay.

The existing seawall structures, with a number of different profiles stitched together including:

- Curved concrete wall with ledge - old and big rocky shore.
- Angled concrete wall.
- Revetment of imported rock rip rap.
- Vertical concrete wall.

Other built development includes

- Bus shelter and bus stop opposite.
- New timber steps down to beach.
- Power poles.

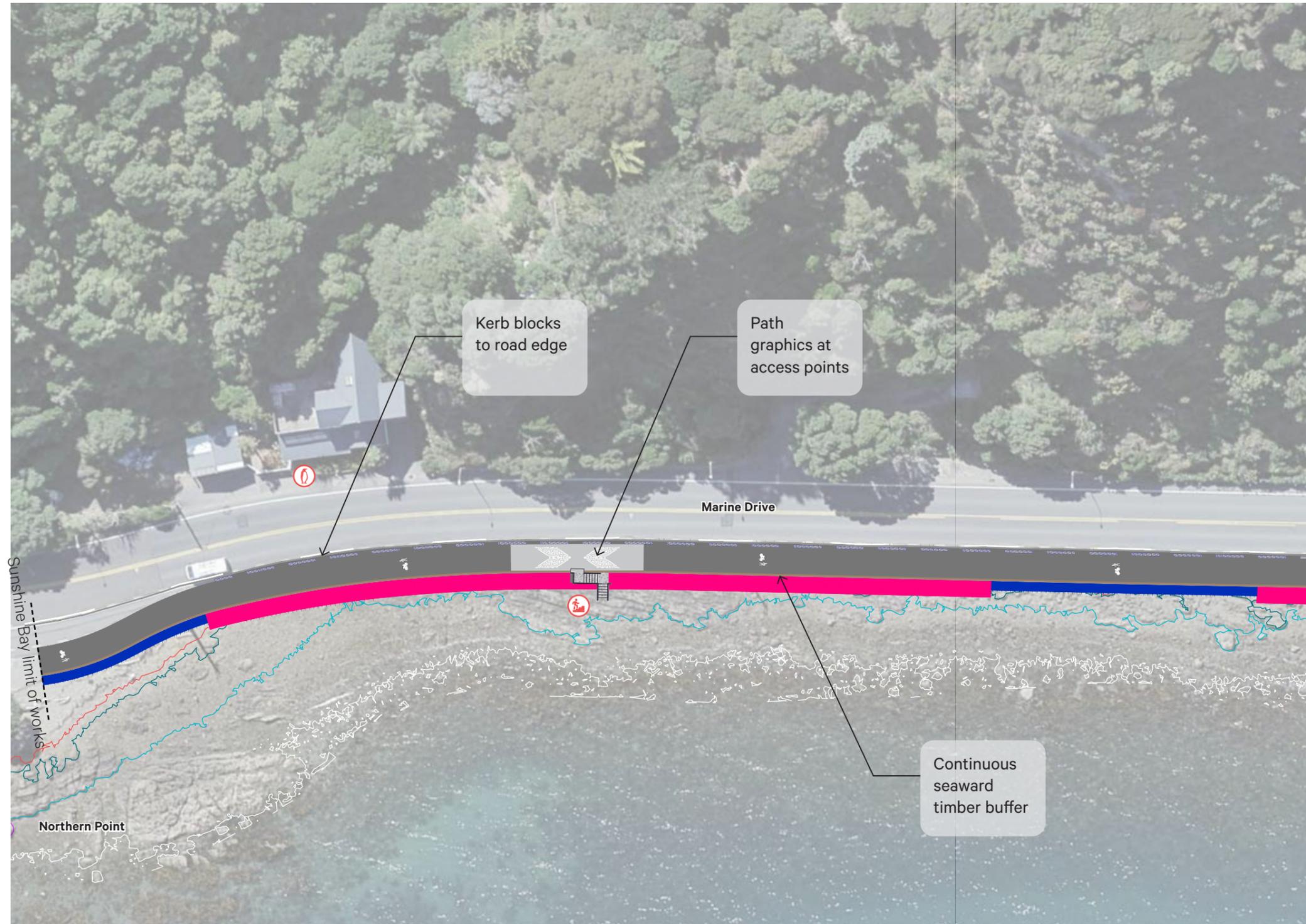
As well as the pohutukawa in the bay, there are clusters of low planting, such as taupata, that have established just below the edge of the road. It is likely that this vegetation regularly washes away in large storms events but re-establishes once a suitable organic base has built up along the coastal edge.

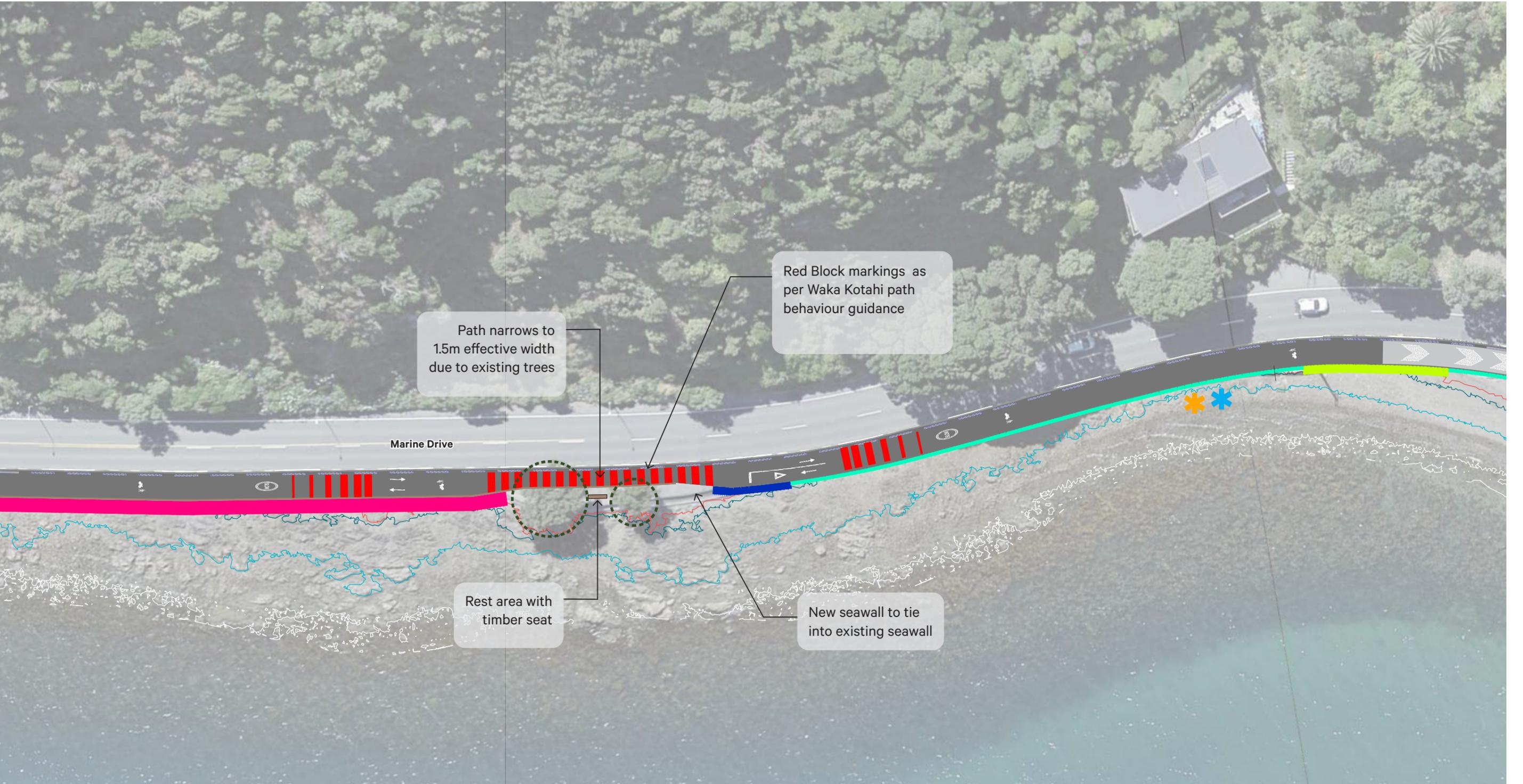
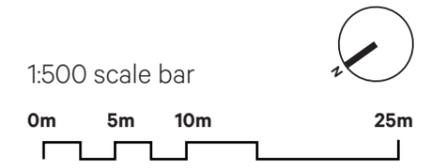
The beach is narrow, wild and exposed, mainly gravelly pebble with a fringe of driftwood and larger rocks. A small fringe of beach is exposed at high tide. At the northern end of the beach, the edge of the path is retained by a 150-200mm timber edge. This is replaced by a low concrete wall, eroded in places and reinforced with loose rip rap, and demolition concrete along the edge of the beach. A set of timber steps has been recently built but it is also possible to pick a route through the rip rap down to the beach.

# Sunshine Bay General Arrangement Plan - North.

Legend.	
	Bus stop
	Parking
	Location of Mini steps
	Location of Standard steps
Seawalls & Path	
	Single curved seawall with bench
	Double curved seawall with bench
	Double curved seawall with Balustrade
	Triple curved seawall with Balustrade
	Shared Path
	Concrete Shared path pavement
	Fall from height barriers
Ecology and Landscape	
	Fish passage- Stream outlet
	Ecological Tiles indicative location
	Known Penguin nesting areas
	Trees retained
	Enhanced planting at headlands
Tides	
	Mean High Water Springs (MHWS)
	High Tide
	Mid Tide
	Low Tide

Wall Type Sections.	
	Single curved seawall with bench
	Double curved seawall
	Double curved seawall with Balustrade
	Triple curved seawall with Balustrade

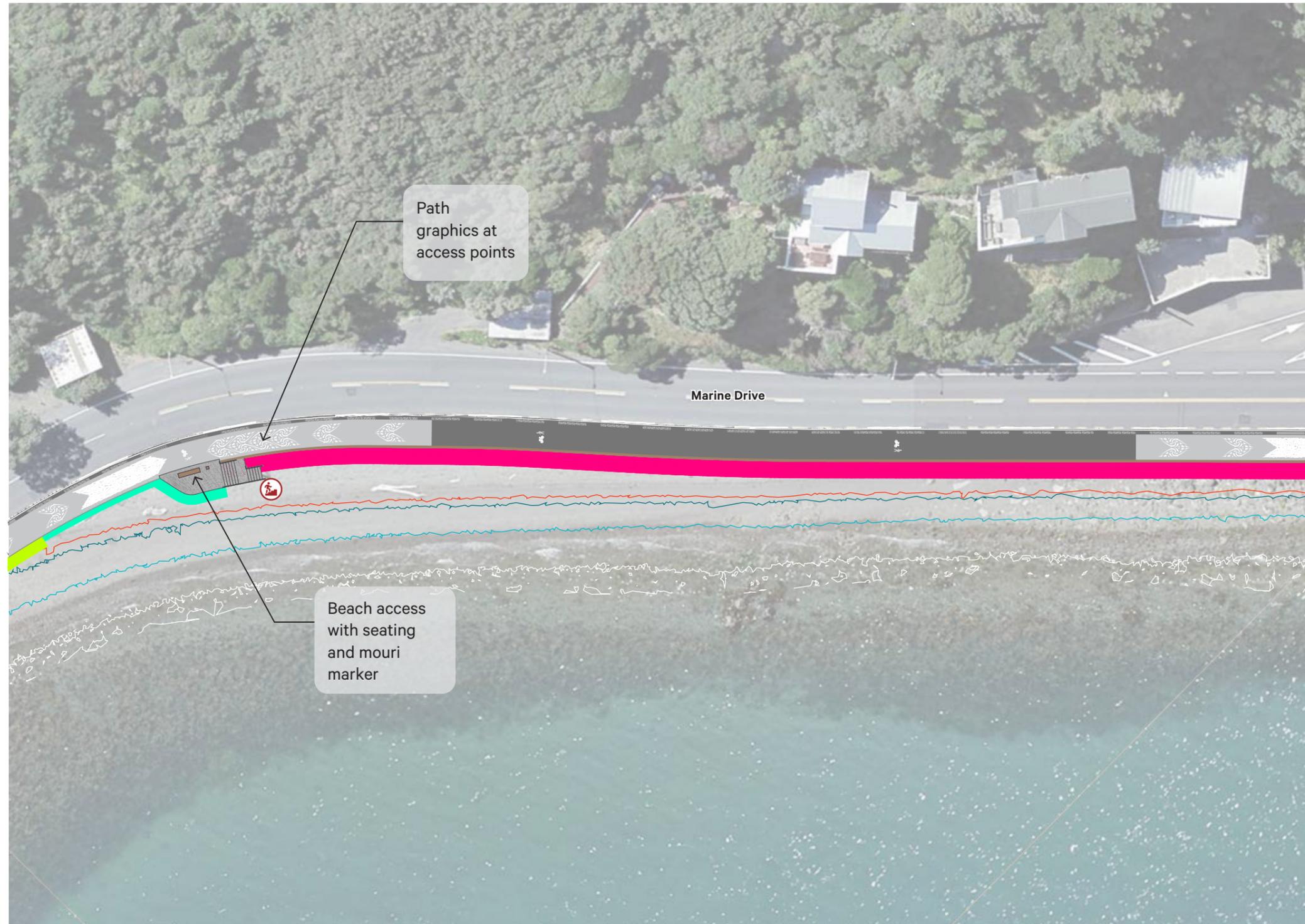




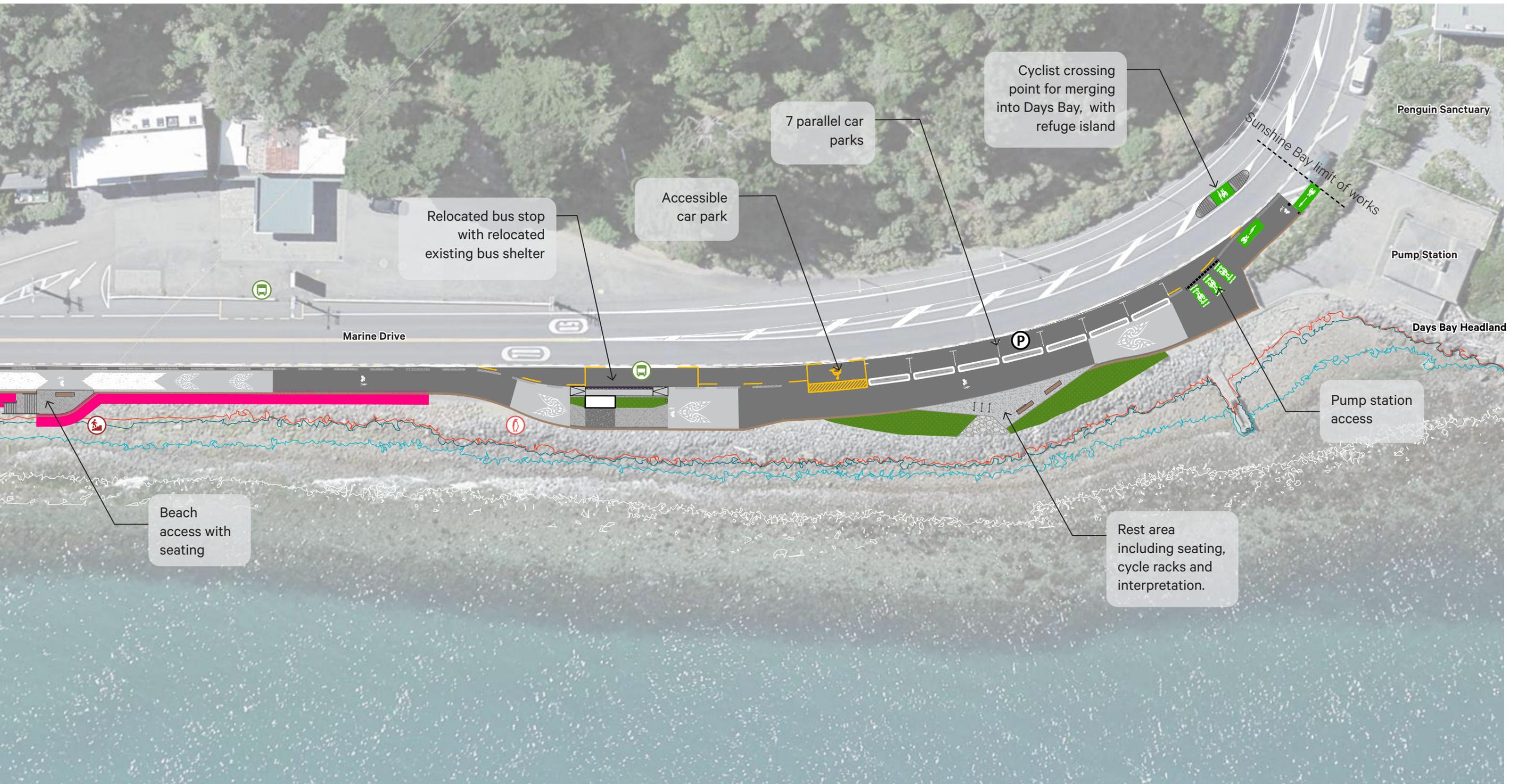
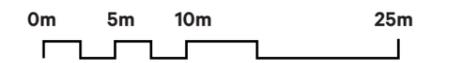
# Sunshine Bay General Arrangement Plan - South.

Legend.	
	Bus stop
	Parking
	Location of Mini steps
	Location of Standard steps
Seawalls & Path	
	Single curved seawall with bench
	Double curved seawall with bench
	Double curved seawall with Balustrade
	Triple curved seawall with Balustrade
	Shared Path
	Concrete Shared path pavement
	Fall from height barriers
Ecology and Landscape	
	Fish passage- Stream outlet
	Ecological Tiles indicative location
	Known Penguin nesting areas
	Trees retained
	Enhanced planting at headlands
Tides	
	Mean High Water Springs (MHWS)
	High Tide
	Mid Tide
	Low Tide

Wall Type Sections.	
	Single curved seawall with bench
	Double curved seawall
	Double curved seawall with Balustrade
	Triple curved seawall with Balustrade



1:500 scale bar



# Sunshine Bay Site Photos.



Figure 1.4 Cars parked along the coastal edge



Figure 1.5 Sunshine Bay bus stop



Figure 1.6 Fishing at Sunshine Bay



Figure 1.7 Sunshine Bay beach



Figure 1.8 Existing seawall to the north of Sunshine Bay



Figure 1.9 Existing revetment with timber stairs

# Approach & Principles.

Rugged Coastal Environment	<ul style="list-style-type: none"> <li>— Reflect the wild coastal character and narrow edge through minimal disturbance and intervention at the coastal edge.</li> <li>— Retain any rocky outcrops.</li> <li>— Sympathetic transitions between sea walls and natural coastal edges.</li> <li>— Retain fishing access at southern end.</li> </ul>
Less is more	<ul style="list-style-type: none"> <li>— Features added minimise obstruction to views and beach access.</li> </ul>
Maintain integrity of rock outcrops	<ul style="list-style-type: none"> <li>— Rock outcrops are remnants of the existing coastal edge.</li> <li>— Retain the natural form of each outcrop.</li> <li>— Where modification is taking place integrate transition from the outcrop to the structure in a natural way.</li> <li>— Retain in-situ as much of the natural colonized rock as possible during sea wall construction.</li> <li>— In addition, reuse the natural colonized rock removed during construction at the base of the sea walls.</li> <li>— The asphalt extent and landscape features proposed at the stopping place just north of the Days Bay Headland sit within the existing constructed envelope, meaning no further encroachment into the existing beach.</li> <li>— Preserve the extent and form of the rock outcrop at the Days Bay Headland - immediately south of the project extent, meaning that the existing pumping station infrastructure, access and landscape features (planting, fencing etc.) will remain in place.</li> <li>— Use natural colonized rock at seawall transitions, particularly those where the concrete sea wall ties back into the natural rocky beach, to integrate the sea wall and eliminate hard concrete edges.</li> </ul>
Retain natural coastal planting	<ul style="list-style-type: none"> <li>— Retain two existing pohutukawa trees between north of the beach.</li> <li>— Retain two existing trees by Northern Point.</li> <li>— Retain and improve planting to headland areas</li> </ul>
<b>Details and elements</b>	
Consistency	<ul style="list-style-type: none"> <li>— Features and elements a consistent suite across the project</li> </ul>
Simple robust forms	<ul style="list-style-type: none"> <li>— Elements such as seating, wheel stops and steps are formed with simple block/rectangular shapes, not to detract from the wild coastal character, yet be simple and accessible to use.</li> </ul>
Existing structures and elements	<ul style="list-style-type: none"> <li>— Existing bus shelter to be retained an integrated into the design.</li> </ul>

Maintenance	<ul style="list-style-type: none"> <li>— The selected materials and patterns are durable, designed with longevity in mind, and that are able to be replicated.</li> <li>— Allow native plant species to self establish where conditions are appropriate.</li> <li>— Work with HCC to understand maintenance requirements.</li> <li>— Relocate electricity poles.</li> <li>— Remove concrete blocks and building rubble, previously used for managing coastal erosion.</li> </ul>
Bay specific narratives	<ul style="list-style-type: none"> <li>— To be undertaken with mana whenua advisors and artists. Cultural expression to integrated into the overall design in relevant areas.</li> </ul>
Materials palette	<ul style="list-style-type: none"> <li>— Hardwood timber - seating, linear barriers, wayfinding marker posts, litter bin palings where required.</li> <li>— Stainless steel - step hand rails, detailing into seating, cycle stands.</li> <li>— Textured concrete - seawall, ecological tiles, tidepools, mini steps.</li> <li>— Asphalt - shared path, and stopping place north of the Day Bay Headland.</li> <li>— Natural colonized rock - sea wall transition points and base of seawall.</li> <li>— Gravel around trees retained</li> </ul>
Plant communities	<ul style="list-style-type: none"> <li>— Enhancement planting to headland areas</li> </ul>

# Priorities for Sunshine Bay.

## Priority 1 - Engineering and Safety

The Design Features Report (Stantec, 2019) sets out engineering requirements for the project. The main points can be summarised as:

- The seawall design allows for adaptive pathways to address sea level rise, such as protection to be added on top of the wall in future as required.
- Achieve consistency in the seawall profile throughout the corridor.
- The seawall is to be constructed from reinforced precast concrete units. Construction methodology of the seawall will be determined by site conditions.
- Resilience of the road and underground services was considered in the design.
- Replacement and extensions to stormwater pipes through the wall are to be like for like, and finish flush with the face of the seawall.
- Seawall transitions to be integrated to avoid abrupt ends/divisions. Transitions between seawall types, e.g. between single and double will be managed between access points (steps and ramps). Transitions between wall edges and the existing coastal edge, e.g. at headlands, will be softened/integrated with natural rock.
- The width of the shared path is to range between 2.5 – 3.5m generally, as per the consented design.
- The path surface is to be mixed asphalt and concrete with a 300mm wide concrete strip on the 'sea side' to define the coastal edge.
- The work is to be an improvement on existing conditions throughout the corridor.
- Fall from height safety will be addressed by either a 1.2m landing, where there is room, or a balustrade where there is not.



Figure 1.10 1.4m Balustrade indicative artistic impression.



Figure 1.11 Seawall indicative artistic impression.

## Priority 2 - Access and Mobility

- Provide a safe walking and cycling facility to connect communities and networks, including CPTED (Safety standards in the National Guidelines for Crime Prevention through Environmental Design in New Zealand) Universal Design Goals shall be used as a basis for decision making.
- Sight lines shall be maintained and unobscured as per CPTED guidelines.
- Fall heights and barriers must comply with NZ Building Code D1 (Access Routes) and F4 (Safety from Falling).
- Provision for safe crossing places and desire lines shall be met as per the Pedestrian Planning and Design Guide (and the Pedestrian Network Guidance (Waka Kotahi, 2021) forthcoming).
- Consideration for the safety for sea birds and animals shall be informed by designed elements and interventions.
- Where stainless steel is used, some minor staining (tea bagging) is likely to occur.
- Continuous timber buffer to seaside (B)
- Continuous Kerb blocks to road edge (A)

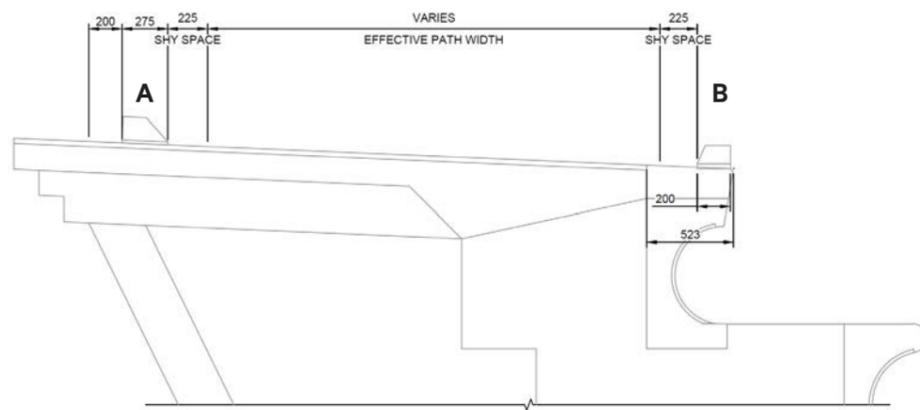


Figure 113 Typical shared path dimensioning



Figure 114 Mini steps, indicative artistic impression.



Figure 112 Standard steps, indicative artistic impression.

# Priorities for Sunshine Bay.

## Ecology

An ecology assessment of intertidal benthic ecology was undertaken in 2016-2017 by EOS Ecology (McMurtrie & Brennan, 2019a). The assessment found that the existing intertidal environment is currently highly modified, with seawalls along the majority of the shoreline consisting primarily of angled concrete seawalls that support low species diversity and richness. Beach areas and fish passage issues have been summarized in the LUDP and BSUDP. Seabird protection is detailed in the Bird Protection Plan.

The main design aspects that will help to improve intertidal ecology and fish passage include:

- A texture applied to the curved seawalls (the curved vertical surface and horizontal flat steps or 'goings'/risers and the vertical sides of access points (steps and ramps) to provide habitat for intertidal biota and splash zone coastal species. The textures are described and shown in the LUDP, the Seawall Revetment Habitat Plan (SRHP), and BSUDPs.
- Ecological tiles applied to discrete locations along the curved seawall and side of some of the access steps that are within the intertidal zone.
- Re-use of in situ rocky material in front of the new curved seawalls.
- Providing for fish passage at stream culverts to ensure the current level of fish passage for fish species migrating into upstream freshwater habitats is maintained or improved.

For sea bird protection areas, the main outcomes are to:

- Use vegetation to provide cover for sea birds, particularly penguins.
- Use fencing to deter people and in pest prevention.
- Use wayfinding signage to support interactive methodologies to tell stories and raise awareness.



**Figure 1.17** Ecological tiles, to be added at discrete locations along vertical surfaces at steps and landings within the intertidal zone.



**Figure 1.15** Sunshine Bay outlet for Sunshine Bay Stream. Fish Passage is proposed for this stream. Ref EOS Ecology



**Figure 1.16** Indicative graphic of texture applied to curved seawalls horizontal surfaces and vertical faces

## Natural Character

The overall adverse effects on natural character for Sunshine Bay are considered to be less than minor for the coastal landscape. The landscape and urban design approach and principles have been developed to mitigate effects of the project on natural character.

As outlined in the LUDP, it is expected that the effects on natural character from the Project, including the seawall and shared path will lessen over time as they weather and become established.

A list of mitigation measures related to natural character attributes is provided below. Further detail of mitigation of effects on natural character and integration with the natural landscape is provided with the description of design areas and elements in the Urban Design Outcomes Section of the LUDP.

Legibility – geomorphology:

- Retention of local rock for reuse at base of the seawalls.

Legibility – wayfinding and orientation:

- Reinforcement of the undulating coastline morphology by positioning the shared path along the coastal edge.
- Opportunities for local variation/reinforcement of local identity in the form of access points from the path to the foreshore.
- Improved access to headlands with strong natural character and natural features (such as trees, rocky outcrops and rock stacks).
- Provision of wayfinding marker, street furniture and signage to reinforce the bays and associated neighbourhoods.
- Provisions for cultural expression and naming to reinforce sense of place.

Visibility – public and private views:

- Consistent detailing along the coastal edge and road edge to reduce the visual impact.
- Appropriate/considered design of urban design and landscape elements such as seating, bins, handrails, seaward side linear barriers, stormwater outlets, planting, signage and path markings to integrate them with the landscape setting.
- Incorporation of eco-mitigation surface textures consistently applied along the lower curve and ‘step’ of the wall to reduce the visual presence of the seawalls.
- Any safety balustrades to be designed as ‘transparent’ as possible to reduce visual appearance.

Picturesqueness:

- Path alignment responds to the local landform and land use patterns.
- Sensitive detailing of urban design and landscape elements, that respond to Mana Whenua, community identity and sense of place.
- Removal of existing unsightly structures and infrastructure along the project site and the replacement of an eroding road with a consistent structurally stable edge.

## Recreational Amenity

The main priorities identified are:

- Creating a fit for purpose shared path that provides access to the coast and to the bays between Point Howard and Eastbourne for people walking and cycling.
- Provide access to the beach, water and headlands.
- Provide stopping and resting places.
- Maintain views to the coast.
- Retain fishing access

# Priorities for Sunshine Bay.

## Cultural Landscape

### Patterns:

*Te Āti Awa tupua rau, he auripo i te manga iti, he auripo i te manga nui raanei, he kaitiaki ki te whenua.*

Te Āti Awa of many phenomena's, where there is a ripple in a small tributary or great river, there is a guardian and protector on the land.

- This speaks to the connection between the tidal movements and the creation of the eastern bays land mass by Tupua Horo Nuku – Ngake

### Mouri Marker:

- The Mouri Marker represents an area of significance to Mana Whenua. It will highlight the Maori and English names of the bays and allow for our cultural narratives of those bays to be told.

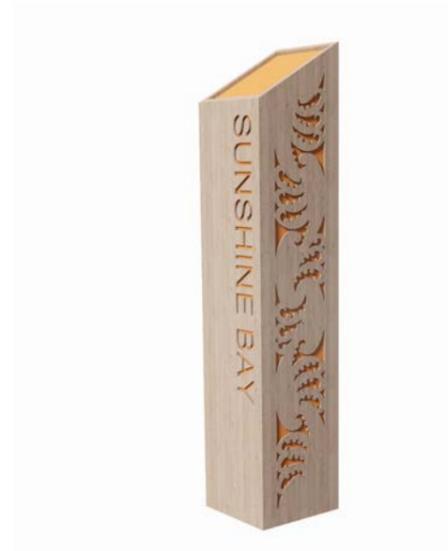


Figure 1.18 Mouri Marker post, indicative sketch.

## Landscape and Urban Design

The LUDP outlines the overarching principles, palettes, and narratives for detailed design. Principles for Sunshine Bay can be summarized as:

- Reflective of rugged coastal environment – materiality, robustness.
- Less is more – emphasise the natural setting and views.
- Maintain integrity of natural rock outcrops.
- Retain and enhance coastal vegetation where possible along coastal edge.
- Consistency across elements – a coordinated suite.
- Simple robust forms.
- Simple colour, surface texture, bespoke signage palettes.
- Maintenance considerations.
- Bay specific narratives expressed through cultural design.
- Sympathetic materials palette.



Figure 1.19 Path graphics, indicative artistic impression



Figure 1.20 Sunshine Bay, indicative artistic impression.

**Ngā mihi nui.**

